

Construction Management Plan

pro forma

100 Avenue Road

By Arada

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Revisions & additional material

Please list all iterations here:

Date	Version	Produced by
03.08.17	1	Essential Living
21.09.17	2	Essential Living

27.09.17	3	Essential Living – further updates from team
09.10.17	4	Essential Living – version published for public consultation
04.12.2017	5	Essential Living – further version published following public meeting and first construction working group
14.03.2018	6	Essential Living
26.04.2018	7	Essential Living
18.07.2018	8	Essential Living
29.08.2018	9	Essential Living – further updates and appendices responding to 19 th July Planning Committee
22.10.18	Approved v.9	Essential Living – Approved in November Committee
04.01.19	10	Essential Living – Addressing PAP
22.01.19	11	Essential Living
26.07.24	12	Arada
20.08.24	13	Arada
21.10.24	14	Arada
14.11.24	15	Arada
10.02.25	16	Arada

Additional sheets

Please note – the review process will be quicker if these are submitted as Word documents or searchable PDFs.

Date	Version	Produced by

Introduction

The purpose of the **Construction Management Plan (CMP)** is to help developers to minimise construction impacts, and relates to both on site activity and the transport arrangements for vehicles servicing the site.

It is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

The completed and signed CMP must address the way in which any impacts associated with the proposed works, and any **cumulative impacts of other nearby construction sites**, will be mitigated and managed. The level of detail required in a CMP will depend on the scale and kind of development. Further policy guidance is set out in Camden Planning Guidance ([CPG](#) [6: Amenity](#) and [CPG](#) [8: Planning Obligations](#)).

This CMP follows the best practice guidelines as described in [Transport for London's](#) (TfL's Standard for [Construction Logistics and Community Safety](#) (**CLOCS**) scheme) and [Camden's Minimum Requirements for Building Construction](#) (**CMRBC**).

The approved contents of this CMP must be complied with unless otherwise agreed with the Council in writing. The project manager shall work with the Council to review this CMP if problems arise during construction. Any future revised plan must also be approved by the Council and complied with thereafter.

It should be noted that any agreed CMP does not prejudice or override the need to obtain any separate consents or approvals such as road closures or hoarding licences.

If your scheme involves any demolition, you need to make an application to the Council's Building Control Service. Please complete the "[Demolition Notice](#)."

Please complete the questions below with additional sheets, drawings and plans as required. The boxes will expand to accommodate the information provided, so please provide as much information as is necessary. It is preferable if this document, and all additional documents, are completed electronically and submitted as Word files to allow comments to be easily documented. These should be clearly referenced/linked to from the CMP. Please only provide the information requested that is relevant to a particular section.

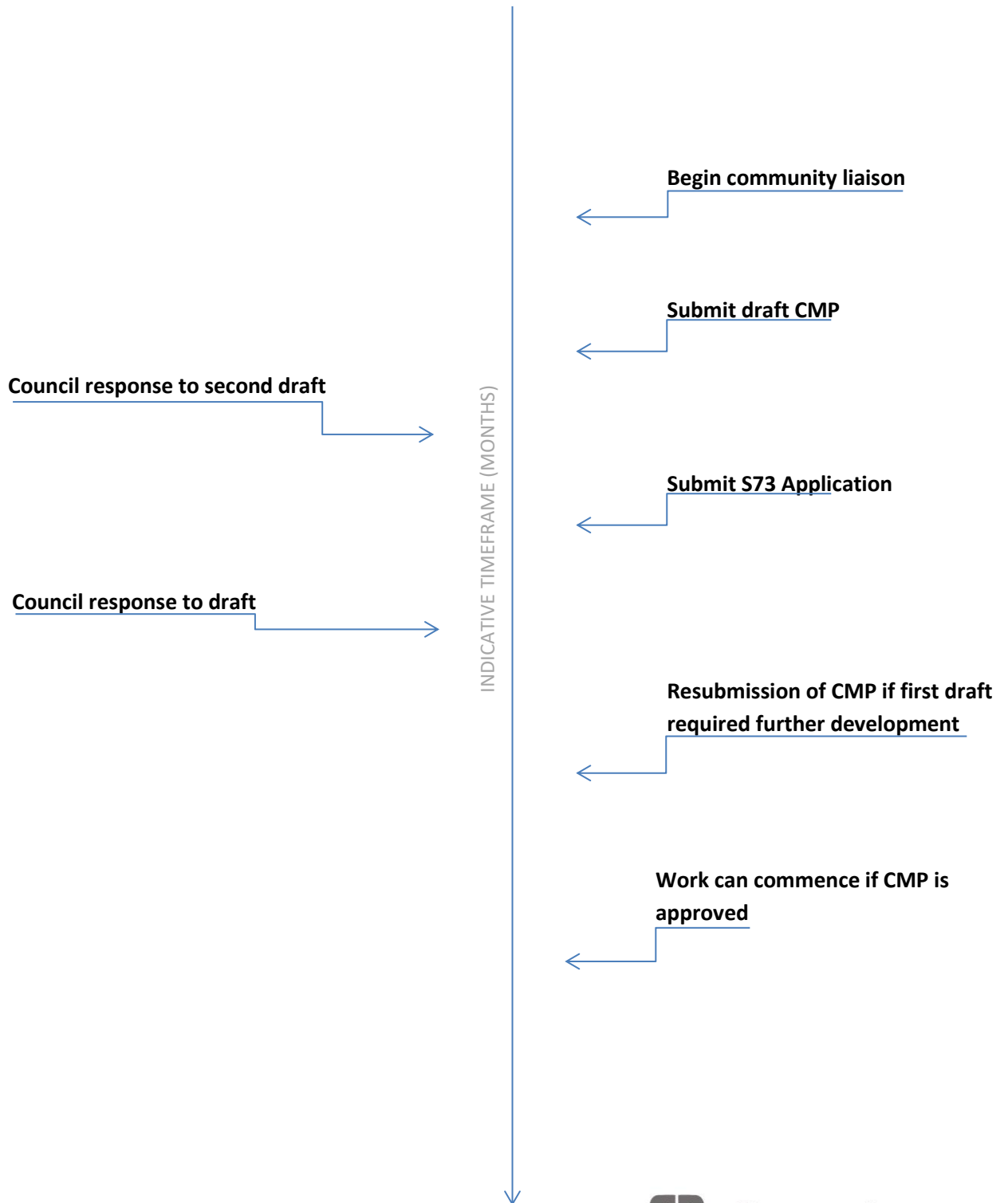
(Note the term 'vehicles' used in this document refers to all vehicles associated with the implementation of the development, e.g. demolition, site clearance, delivery of plant & materials, construction etc.)

Revisions to this document may take place periodically.

Timeframe

COUNCIL ACTIONS

DEVELOPER ACTIONS



Contact

1. Please provide the full postal address of the site and the planning reference relating to the construction works.

Address: 100 Avenue Road, London, NW3 3HF

Planning reference number to which the CMP applies: TBC

2. Please provide contact details for the person responsible for submitting the CMP.

Name: Gabriele Tomassini

Address: 4-5 Coleridge Gardens, London, NW6 3QH

Email: gabriele.tomassini@regal.co.uk

Phone: 020 7328 7171

3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses.

Name: Simon Bram

Address: 4-5 Coleridge Gardens, London, United Kingdom, NW6 3QH

Email: simon.bram@regal.co.uk

Phone: 07958132029

4. Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and businesses if different from question 3. In the case of the Community Investment Programme (CIP), please provide the contact details of the Camden officer responsible.

Name: Cavendish Consulting

Email: hannah.anderson-jones@cavendishconsulting.com

Phone: [01962 893 856](tel:01962893856)

5. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Name: Regal One London Construction Ltd

Address: 4-5 Coleridge Gardens, London, United Kingdom, NW6 3QH

Email: TBC

Phone: TBC

Site

6. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies.

Existing Site:



The site is located immediately off the A41 and College Crescent, in the Swiss Cottage area of the London borough of Camden. The site currently has completed the piling and the basement box. Construction is not in progress.

The site has several existing uses immediately adjacent to it; to the East is an area of Camden Park Lands and Hampstead Theatre. To the south is also a small strip of Camden Park Land followed by Swiss Cottage Leisure Centre. To the west is a major road interchange followed by several commercial units. To the north is the entrance to Swiss Cottage Underground Station followed by The Royal Central School of Speech and Drama. Going further afield to the East and West are mostly residential areas, with commercial units following the main transport high North and South.

Site location & Surrounding Area:

Proposal

Demolition of existing building (completed) and redevelopment (in progress) of a 26 storey building and a part 8, part 6, storey building comprising a total of 236 residential units (class C3) and up to 1,188 sqm of flexible retail/financial or professional or café/restaurant floorspace (classes A1/A2/A3) inclusive of part sui generis floorspace for potential new London Underground station access fronting Avenue Road and up to 1,372sqm for community use (class D1) with associated works including enlargement of existing basement level to contain disabled car parking spaces and cycle parking, landscaping and access improvements.

Proposed Visual:



7. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings etc).

Construction works proposed for the above description of development:

Demolition began Q4 2018 and is now complete. Piling and Basement Box works were also completed in Q3 2020.

Issues and challenges for the construction works will include:

- Theatre next door which may necessitate restricted working hours for noisy works
- The Royal Central School of Speech and Drama located to the north of the site
- Residential access to north east
- Busy red route to south east
- Public Park to the East and South
- Creating multiple safe access points to the site to minimise the impact on the public
- Temporary Bus stop relocation
- Signals relocation
- Working close to the Swiss Cottage underground station

8. Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting etc.).

ID	Location	X	Y	Distance	Risk Theme
1	17-24 Finchley Road	526632	184222	107m	Site Traffic
2	115-121 Finchley Road	526632	184259	87m	Site Traffic
3	Cresta House	526623	184299	89m	Site Traffic
4	139 Finchley Road	526616	184348	111m	Site Traffic
5	Northways Parade	526642	184403	130m	Site Traffic
6	Royal Central School of Speech & Drama (1)	526687	184363	74m	Site Traffic, Noise, Dust
7	Royal Central School of Speech & Drama (2)	526710	184386	94m	Site Traffic, Noise, Dust
8	Embassy Theatre	526725	184390	98m	Site Traffic, Noise, Dust
9	Market	526716	184364	72m	Site Traffic, Noise, Dust
10	Hampstead Theatre - west boundary	526721	184341	49m	Site Traffic, Noise, Dust
11	Hampstead Theatre - south boundary	526750	184336	57m	Site Traffic, Noise, Dust
12	Underground Station - Eton Avenue entrance	526686	184332	47m	Site Traffic, Noise, Dust
13	Underground Station - Avenue Road entrance	526701	184287	12m	Site Traffic, Noise, Dust
14	Temporary bus stop lay-by	526670	184323	52m	Site Traffic
15	Ye Old Swiss Cottage pub	526685	184248	52m	Site Traffic
16	Odeon Cinema	526699	184218	76m	Site Traffic
17	Central library	526761	184209	96m	Site Traffic
18	Leisure centre	526782	184220	101m	Site Traffic
19	Tennis courts	526777	184248	79m	Location
20	Community centre	526828	184265	119m	Site Traffic
21	Park (centre of)	526772	184296	60m	Location
22	21A Winchester Road	526815	184292	103m	Site Traffic
23	23 Winchester Road	526810	184302	99m	Site Traffic
24	25 Winchester Road	526807	184308	96m	Site Traffic
25	27-29 Winchester Road	526803	184313	94m	Site Traffic
26	31 Winchester Road	526800	184320	92m	Site Traffic
27	33 Winchester Road	526796	184326	91m	Site Traffic
28	35 Winchester Road	526793	184332	90m	Site Traffic
29	Mora Burnet House	526778	184357	92m	Site Traffic
30	Swiss Cottage School	526889	183997	344m	Site Traffic
31	UCL Academy	526865	184120	231m	Site Traffic

Shown in Appendix P

9. Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents and proposed site access locations.

Due to the installation of a required “pit lane” to aid deliveries and the southern access point onto site, bus stop D on Avenue Road needs to be relocated for the duration of the construction of the project on Adelaide Road. This bus stop will be of a similar size to the current stop.

To the north of the site is an access point for deliveries to the basement via Eton Avenue using the existing car park ramp. The existing car park entrance will still be utilised for the remainder of the project as this is the only entrance to the basement.

The second entrance from Avenue Road allows vehicles to exit the project from the southern section of the site.

A third access point has been installed to the north of the LUL entrance on the A41. The crossover and any associated changes within Avenue Road will be implemented prior to any works commencing on site. This access point will allow the safe access of vehicles to site from the A41, negating higher numbers of vehicle movements that would otherwise be necessary to the northern access point.

The northern boundary will be amended in Q3 2027 to allow pedestrian access to the ground floor of the tower which will be utilised as a marketing suite for sales purposes. The hoarding will locally return into the building line with temporary doors and windows installed with the external area being paved and landscaped.

A full set of drawings detailing the existing and proposed highway network layout including the “pit lane” and bus stop relocation are attached at Appendix A.

10. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale.

A draft construction programme is provided at Appendix B. Please refer to that for full details.

The proposed construction start date is March 2026 and the construction work is anticipated to last approximately 135 weeks.

11. Please confirm the standard working hours for the site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

12. Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

The existing utilities impacted by the works have been assessed by Arada along with an assessment of what needs to be diverted and what new supplies are needed. Set out in Appendix C is a schedule of proposed utilities works and a plan of the existing utilities around the site.

Community Liaison

A neighbourhood consultation process must have been undertaken prior to submission of the CMP first draft. This consultation must relate to construction impacts, and should take place following the granting of planning permission in the lead up to the submission of the CMP. A consultation process specifically relating to construction impacts must take place regardless of any prior consultations relating to planning matters. This consultation must include all of those individuals that stand to be affected by the proposed construction works. These individuals should be provided with a copy of the draft CMP, or a link to an online document. They should be given adequate time with which to respond to the draft CMP, and any subsequent amended drafts. Contact details which include a phone number and email address of the site manager should also be provided.

Significant time savings can be made by running an effective neighbourhood consultation process. This must be undertaken in the spirit of cooperation rather than one that is dictatorial and unsympathetic to the wellbeing of local residents and businesses.

These are most effective when initiated as early as possible and conducted in a manner that involves the local community. Involving locals in the discussion and decision making process helps with their understanding of what is being proposed in terms of the development process. **The consultation and discussion process should have already started, with the results incorporated into the CMP first draft submitted to the Council for discussion and sign off.** This communication should then be ongoing during the works, with neighbours and any community liaison groups being regularly updated with programmed works and any changes that may occur due to unforeseen circumstances through newsletters, emails and meetings.

Please note that for larger sites, details of a construction working group may be required as a separate S106 obligation. If this is necessary, it will be set out in the S106 Agreement as a separate requirement on the developer.

Cumulative impact

Sites located within high concentrations of construction activity that will attract large numbers of vehicle movements and/or generate significant sustained noise levels should consider establishing contact with other sites in the vicinity in order to manage these impacts.

The Council can advise on this if necessary.

13. Consultation

The Council expects meaningful consultation. For large sites, this may mean two or more meetings with local residents **prior to submission of the first draft CMP**. Please ensure that any changes to parking and loading on the public highway are reflected in the consultation. Please agree highways set up plans in advance with Camden if there is any uncertainty with this.

Evidence of who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation should be included. Details of meetings including minutes, lists of attendees etc. should be appended.

In response to the comments received, the CMP should then be amended where appropriate and, where not appropriate, a reason given. The revised CMP should also include a list of all the comments received. Developers are advised to check proposed approaches to consultation with the Council before carrying them out. If your site is on the boundary between boroughs then we would recommend contacting the relevant neighbouring planning authority.

Please provide details of consultation of the draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.

Consultation: Historical, Current and Future

Table below outlines all consultation meetings undertaken by the previous developer (up to 2020) and by Arada Avenue Road Limited from 2024 onwards (meeting minutes and consultation records are included at appendix D along with the Consultation Statement from Polity where the outcome of the public consultation has been recorded and attached always at appendix D).

Consultation is ongoing and will continue through the S73 planning process and into the construction phase.

Party Consulted	Date	Topic	Outcome
LUL	17/06/2014	Location of basement in proximity with LUL stair case	EL to progress planning drawings expected Q3 2015
LUL	03/07/2014	Movement monitoring of LUL tunnels	URS to progress movement monitoring and condition surveys

LUL	06/08/2014	Monitoring Tender Review	URS recommending letter of intent from LUL to maintain programme
LUL	29/04/2019	List of various points for consideration when working close to LUL asset	LUL confirmed the following: <ul style="list-style-type: none"> - No oversailing of crane over station entrance - Traffic management on Avenue road needs to be rigorous - Concept design statement needs to be completed by Mace - Monitoring Action Plan to be completed by Mace Load assessment required to be completed of LUL asset
LUL	06/09/2019	License to carry out works for protection deck	LUL issued approved license
LUL	06/09/2019	Concept Design Statement	LUL issued approval following review of JFH CDS
LUL	14/09/2019	Party Wall Award	Agreed and signed by LUL Party Wall Surveyors
TfL	05/06/2017	CS11(Programme)	TfL to review EL development with the CS11 programme and phasing.
TfL	07/06/2017	CS11 (site Logistics)	EL to advance site logistic plans at SPA's for further review with TfL
TfL	26/09/2017	CMP and S278	TfL happy with the logistics plan in principal. To be agreed following further input. TfL proposed a working group of all construction projects in the vicinity to ensure all potential issues are addressed throughout the project.
TfL	29/07/2018	CMP and CS11	Confirmation to proposed CMP submitted by EL and acknowledgement that the document is a live document and

			may need to change depending upon CS11
TfL	22/08/2018	CMP	Confirmation that TfL will speak at the planning committee in support of the CMP.
TfL	13/08/2018	CS11	Confirmation that CS11 JR was successful
TfL	03/04/2019	Pit Lane	Confirmation that EL can proceed with the insertion of the Pit Lane
TfL	May to Sept 2019	Pit Lane	Discussions and detailed design with TfL regarding the pit lane
TfL	18/09/2019	Pit Lane	Issue of the permit to insert the Pit Lane
Camden Parks	06/10/2017	Impact of the project on the adjacent park	EL issue logistics plans and Camden Park to seek advice from legal team in removal of trees and artwork
Camden Parks	11/10/2018	Parkland	Public Consultation event held regarding the use of the parkland for the construction of 100 Avenue Road
Camden Parks	02/10/2019	Parkland	Completion of the parkland licence
Meeting with HS2	16/10/2017	To understand any interfaces with the HS2 scheme and the Theatre Square scheme	The outcome of the meeting was that the timing of the HS2 works would not impact the Theatre Square scheme and vice versa
HS2 and Chalcott Estate	Monthly	To understand any interfaces with the HS2 scheme, Chalcott works and the Theatre Square scheme	Monthly meetings to discuss work programme and vehicle movements
Public Consultation Meeting at Harris School	19/10/2017	Public Consultation meeting to present the CMP	Feedback was received and has been collated into the report by Polity attached at appendix D
Meeting with Camden Environmental Health Officer	10/11/2017	Seek feedback and comment from the Environmental Health Officer	Comments have been incorporated into v.5 of the CMP

First Working Party Meeting with residents	15/11/2017		The meeting was attended by over 50 people and did not constitute a formal working party meeting. Further feedback was recorded which has been collated into the Polity report attached and appendix D
Further public consultation & presentation of v.5 of the CMP	29/11/2017	Presentation of updated details forming v.5 of the CMP	<p>The meeting was attended by c.18 members of the public and local groups. Record in the Polity report attached at appendix D</p> <p>The key issue that was being raised by the local groups was the traffic generation and the use of Winchester Road. We have reviewed the use of Winchester Road and will cap the total trips per day to 14 and will stop using Winchester Road by month 29 of the project</p>
Construction Working Groups	Dec 2018 – March 2020	Monthly CWG meetings	Regular meetings with representatives from resident groups and local businesses to discuss stages of work that have been undertaken and that will be undertaken.
Camden Open Spaces	08/08/2024	Presentation of landscape layout	Discussion around integration of landscape design within the wider Camden park regeneration scheme. Mention of re negotiation of the parkland license to use part of the park for the south site gate
St Johns Wood Square Project Site	September '24	Site traffic coordination	Introduction and exchange of logistics plans. Agreement that the two sites do not impact each other but that there needs to be coordination when large deliveries are expected.

LUL	October '24	Licenses to carry out work	Introduction to the LUL asset management officer and discussion of next steps towards securing licenses to protect the two LU entrances.
Swiss Cottage Market Consultation	01/10/2024	Deborah Carpenter (Market Development Manager) and Joze Stivan (Senior Transport Planner)	Discussion around market operating hours and coordination with residential deliveries once construction is complete. Arada confirmed that extant CMP guidelines will be followed with construction vehicles servicing the site from Avenue Rd.
Royal Central School of Speech and Drama Meeting	11/03/2025	Introduction of the School's development project. Presentation of Arada DAS and CMP.	School site walk around, discussion on demo and construction logistics and whether the school's operations might clash with Arada's early enabling works. Presentation of Arada's project and discussion around servicing of site via Avenue Road.

14. Construction Working Group

For particularly sensitive/contentious sites, or sites located in areas where there are high levels of construction activity, it may be necessary to set up a construction working group.

If so, please provide details of the group that will be set up, the contact details of the person responsible for community liaison and how this will be advertised to the local community, and how the community will be updated on the upcoming works i.e. in the form of a newsletter/letter drop, or weekly drop in sessions for residents.

From the outset of their discussions with Camden Officers in 2017, Essential Living carefully reviewed the Council's Construction Management (CMP) Pro Forma and its Community liaison guidance: guidance for developers and contractors. This guidance has continued to be reviewed during the various phases of work and when considering amendments to the CMP.

As stated in the Camden CMP Proforma, the CMP is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

Following discussions with Officers it was agreed that a public consultation event to allow the public to comment on the previous proposed amendment to the CMP is appropriate. In addition, Essential Living did add an extra consultation event prior to the submission to allow a further opportunity for the public to input to the document.

Invites were sent to all parties who previously commented on the CMP via e-mail (200+) along with a mailshot to the area previously agreed (see appendix D of the original CMP v11) with officers.

Essential Living had been running monthly Construction Working Group meetings (CWGs) since December 2018, when demolition commenced. The membership of the Group was agreed with Camden officers, with on-going opportunities to make amendments to membership at the request of ward councillors.

The group is made up of:

Constituted Residents' Groups –

- Belsize Residents Association (Prabhat Vaze)
- Save Swiss Cottage (Janine Sachs)
- Cresta House Residents Association (Edie Raff)
- Winchester Road Residents Association (Elaine Chambers)
- Combined Residents Associations of South Hampstead (CRASH – Peter Symonds)
- Taplow Tenants & Residents Association (no named representative)
- Visage Building (Alex Andrews)
- Resident of Eton Court (Kitty Balint)
- Belsize Village Residents and Traders' Association (James Brown)
- Belsize Conservation Area Advisory Committee (David Thomas)
- Hampstead District Management Committee (Gianni Franchi)
- The Belsize Society
- Primrose Hill Court Tenants and Residents Association
- Netherhall Neighbourhood Association (Jenny White)
- Save Swiss Cottage Action Group (David Reed)

Business/Commercial interests/Cultural/Educational –

- Hampstead Theatre

- Swiss Cottage Market
- Central School of Speech and Drama
- The Winch
- Swiss Cottage Community Centre
- Swiss Cottage Central Library
- Representative of businesses on Winchester Road
- Swiss Cottage Surgery
- Camden Market

Local Councillors –

- Councillor Ajok Athian (Labour, Primrose Hill)
- Councillor Anna Burrage (Labour, Primrose Hill)
- Councillor Matt Cooper (Labour, Primrose Hill)
- Councillor Judy Dixey (Liberal Democrat, Belsize)
- Councillor Matthew Kirk (Liberal Democrat, Belsize)
- Councillor Tom Simon (Liberal Democrat, Belsize)
- Councillor Nina De Ayala Parker (Labour, South Hampstead)
- Councillor Tommy Gale (Labour, South Hampstead)
- Councillor Izzy Lenga (Labour, South Hampstead)

In addition to the CWG, EL produce bi-weekly dashboards that are distributed to list of registered interested parties including the attendees of the CWG meetings and ward Councilors. Key elements here include:

- Construction updates including work undertaken and upcoming
- Register of vehicle movements comparing actual with permitted
- Forecast vehicle movements
- Environmental summaries
- Website with regular updates and links to live monitoring
- Recording complaints in a register
- Notice board on the external boundary of the site updated on a by monthly basis

15. Schemes

Please provide details of your Considerate Constructors Scheme (CCS) registration. Please note that Camden requires [CCS site registration](#) for the full duration of your project including additional [CLOCS visits](#) for the full duration of your project. Please provide the CCS site ID number that is specific to the above site. A company registration will not be accepted, the site must be registered with CCS.

Be advised that Camden is a Client Partner with the Considerate Constructors Scheme and has access to all CCS inspection and CLOCS monitoring reports undertaken by CCS.

Contractors will also be required to follow the [Guide for Contractors Working in Camden](#). Please confirm that you have read and understood this, and that you agree to abide by it.

The scheme will be registered with CCS prior to any works commencing on site. Regal One London Construction will adhere to all CCS codes and policies and will aim for an outstanding score.

16. Neighbouring sites

Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site. The council can advise on this if necessary.

- HS2 construction at Cardington Street
- Chalcott Estate recladding and refurbishment
- St Johns Wood Barracks
- O2 Finchley Road
- The Royal Central School of Speech and Drama

Essential Living had undertaken regular consultation with both the managers of the refurbishment work of the Chalcott Estate and HS2 to understand the respective programme of works of the projects.

The Chalcott Estate will be serviced via both Fellows Road and Winchester Road, whilst HS2 will be utilising Adelaide Road. The introduction of the additional access point for the 100 Avenue scheme and removal of all non-planned movements along Winchester Road will substantially reduce the potential impact of vehicle movements with the other projects planned in the area

St Johns Wood Barracks main access route is down Finchley Road, turning onto Queens Terrace and into site via gates off Queens Terrace. A second route is Finchley Road turning down Acacia Avenue and then left up Ordnance Hill and into site via gates on Ordnance Hill. Exit routes are generally out Ordnance Hill up onto St Johns Wood Park and onto Adelaide Road.

Transport

This section must be completed in conjunction with your principal contractor. If one is not yet assigned, please leave the relevant sections blank until such time when one has been appointed.

Camden is a CLOCS Champion, and is committed to maximising road safety for Vulnerable Road Users (VRUs) as well as minimising negative environmental impacts created by motorised road traffic. As such, all vehicles and their drivers servicing construction sites within the borough are bound by the conditions laid out in the CLOCS Standard.

This section requires details of the way in which you intend to manage traffic servicing your site, including your road safety obligations with regard to VRU safety. It is your responsibility to ensure that your principal contractor is fully compliant with the terms laid out in the CLOCS Standard. It is your principal contractor's responsibility to ensure that all contractors and sub-contractors attending site are compliant with the terms laid out in the CLOCS Standard.

Checks of the proposed measures will be carried out by CCS monitors as part of your CLOCS monitoring visits through CCS and possibly council officers, to ensure compliance. Please refer to the CLOCS Standard when completing this section.

Please contact CLOCS@camden.gov.uk for further advice or guidance on any aspect of this section.

Please note that this section may also be referred to as a Construction Logistics Plan in the context of the CLOCS Standard.

CLOCS Contractual Considerations

17. Name of Principal contractor:

Regal One London Construction Ltd.

18. Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract.

Arada and Arada One London Construction will only use Subcontractors and Suppliers that are members of the Fleet Operator Recognition Scheme (FORS) and accredited with a minimum of silver standard. By only using such sub-contractors and suppliers we will be working with organisations that are CLOCS compliant.

At Arada, we will ensure our contractors put in place the following checks:

1. Contractual
 - a. At tender stage, we stipulate the requirement for contractors to be members of FORS and have a minimum of Silver standard.
 - b. At mid tender interviews with CMP is reviewed and the need for FORS accreditation and evidence of compliance with the CLOCS Standard.
 - c. Noncompliance with the CLOCS Standard and a lack of evidence that a contractors' vehicle fleet is FORS accredited at Silver will result in the contract not being awarded.
 - d. Arada and their contractors have made the amendment to include the need for FORS Silver.
2. Site Set Up
 - a. The CMP will be communicated to the sub-contractor(s) and supplier(s) at pre-contract order meeting and pre-commencement meeting to ensure that they are aware of specific constraints of the site in connection with; access routes, delivery times, booking deliveries, compliance with the traffic marshals instructions and only parking in the designated loading and unloading areas
 - b. Our construction phasing plan will be issued all sub-contractors and suppliers.
3. Site Operations
 - a. There will be continued reinforcement of the requirements of the CMP in connection with delivery times and routes and non-compliance will be policed with a warning system and result in persistent offenders being barred from the site.
 - b. When there are requirements for any special deliveries to site such as early mornings of out of hours then permission will be sought from Camden and the residents informed via the Construction Working Group.
 - c. The Project Manager will keep a log of all deliveries with compliance check to ensure that delivery lorries are FORS registered – the log will be submitted to Camden on a monthly basis.
4. Vehicle and Vehicle Operator Check
 - a. All vehicles arriving at site will be checked to be at FORS Silver as a minimum and those that are not will be turned away.
 - b. At the site gate, the driver will be asked to present their certificate or card to confirm that they have had vulnerable road user safety training.

19. Please confirm that you as the client/developer and your principal contractor have read and understood the CLOCS Standard and included it in your contracts.

I confirm that I have included the requirement to abide by the CLOCS Standard in my contracts to my contractors and suppliers.

Site Traffic

Sections below shown in blue directly reference the CLOCS Standard requirements. The CLOCS Standard should be read in conjunction with this section.

20. Traffic routing: *“Clients shall ensure that a suitable, risk assessed vehicle route to the site is specified and that the route is communicated to all contractors and drivers. Clients shall make contractors and any other service suppliers aware that they are to use these routes at all times unless unavoidable diversions occur.”* (P19, 3.4.5)

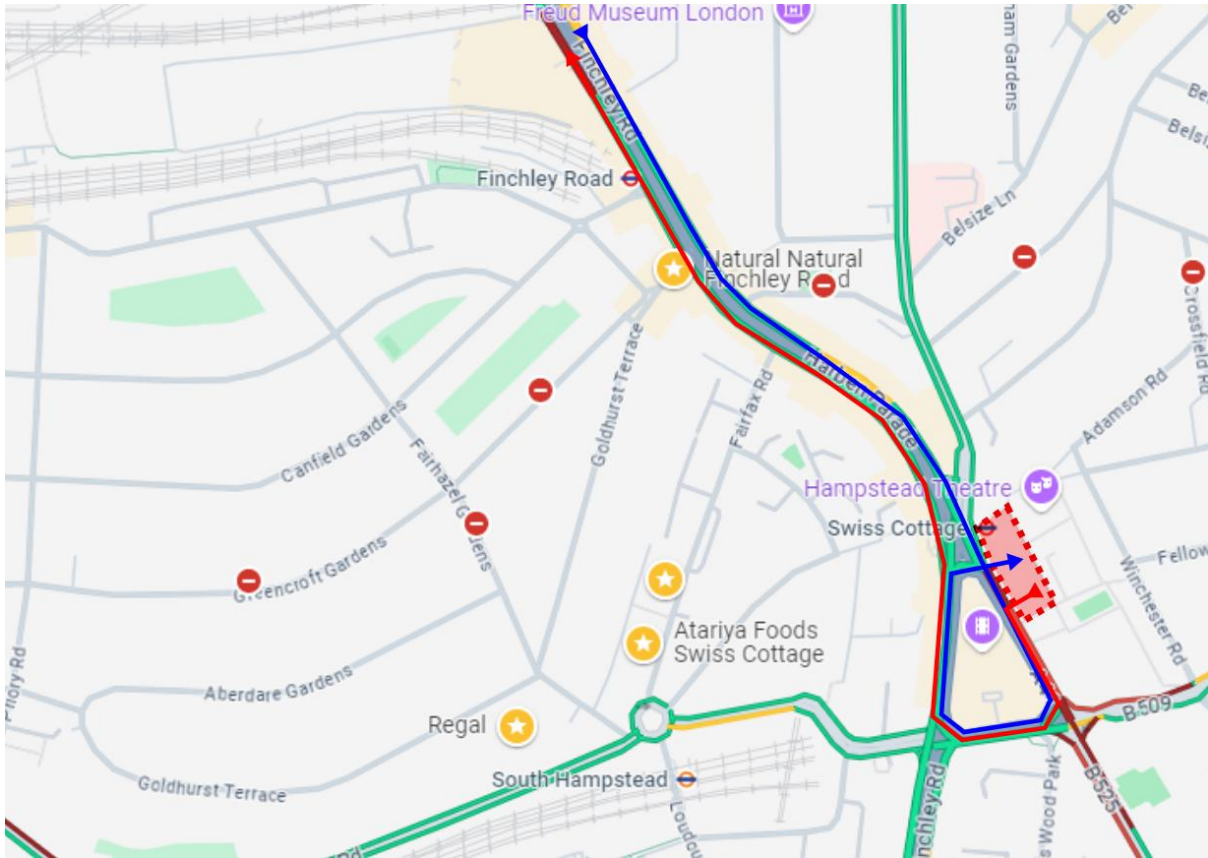
Routes should be carefully considered, and risk assessed, taking into account the need to avoid where possible any major cycle routes and trip generators such as schools, offices, public buildings, museums etc. Where appropriate, on routes that use high risk junctions (i.e. those that attract high volumes of cycling traffic) installing Trixi mirrors to aid driver visibility should be considered.

Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. The route(s) to and from the site should be suitable for the size of vehicles that are to be used.

- a. Please indicate routes on a drawing or diagram showing the public highway network in the vicinity of the site including details of how vehicles will be routed to the [Transport for London Road Network](#) (TLRN) on approach and departure from the site.

Site entry points

Due to the location of the site all construction traffic approaches the site from the north following the A41. The A41 is a major road and part of the TLRN (Transport for London Road Network). The primary access point to the site is from the A41 where there are two access gates for construction vehicles to enter and exit the site in a safe one-way gyratory action. In addition to these access points, there is a pit lane which takes the inner lane of the A41 which has space for two construction vehicles.



There is an existing entrance to the already built basement in Eton Avenue, deliveries through this access will only be requested in exceptional circumstances. Construction traffic will be instructed to not approach from any other route.

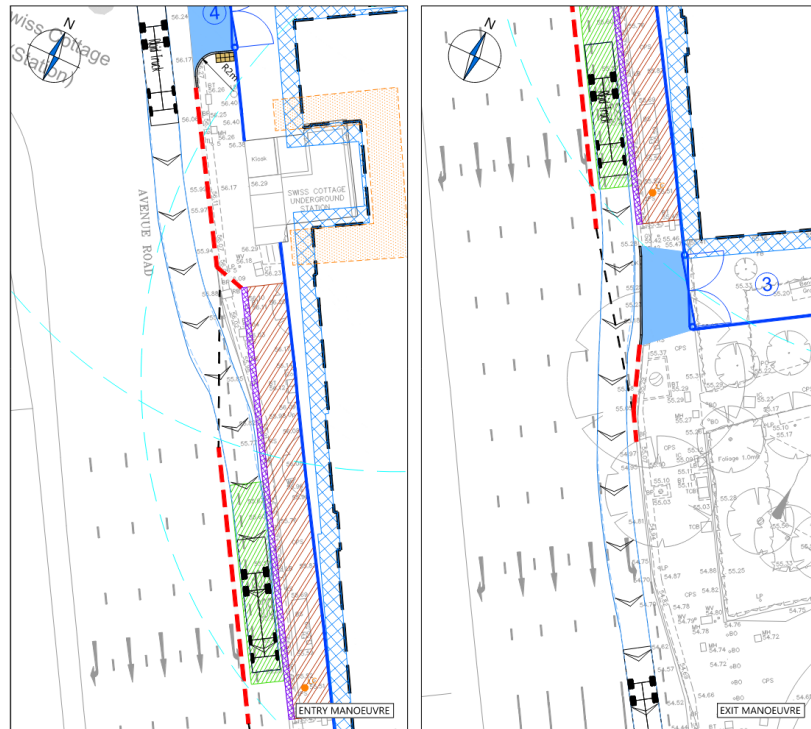


Existing entrance into basement car park shown above from Eton Ave to be utilised for basement deliveries only.

Access Routes 1 and 2

Access and egress using the Pit Lane (Route 1):

Vehicles will travel along Finchley Road/ A41 onto Avenue Road A41 and into the Pit Lane. Vehicles exit the Pit Lane by turning right onto Adelaide Road, turn right again onto Finchley Road and head back out of London via the A41.



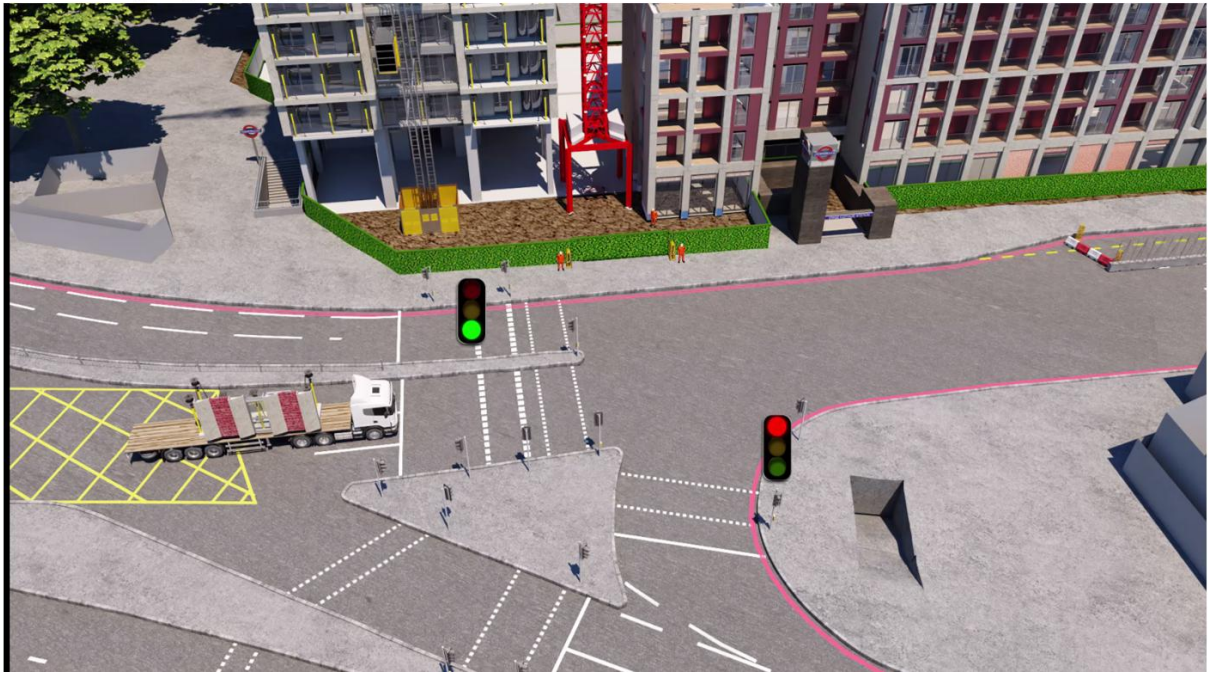
Route 1 - Pit Lane swept path analysis

Northern Crossover on site/ southern crossover exit A41 (Route 2):

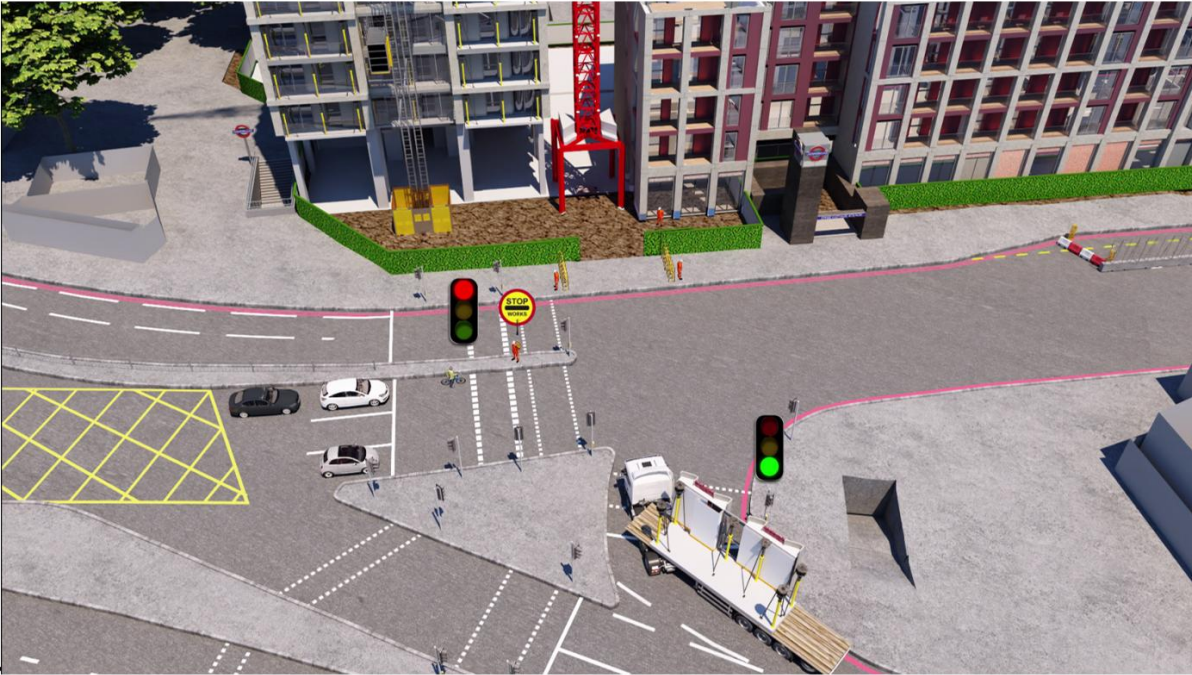
Entry to crossover vehicles will travel along Finchley Road/ A41 onto Avenue Road A41, turn right onto Adelaide Road, turn right again onto Finchley Road and a final right across the A41 into the northern crossover into the construction project. Vehicles exit the construction site by exiting the southern gate turning left onto Avenue Road then turning right onto Adelaide Road, turn right again onto Finchley Road and head back out of London via the A41.

C

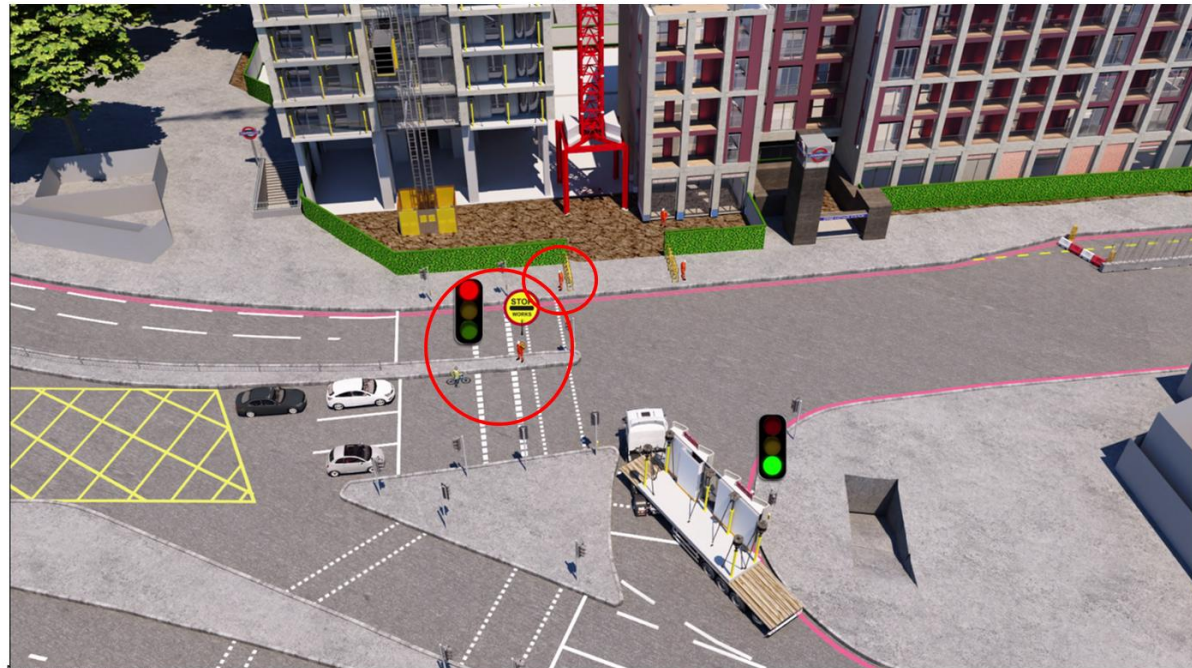
2. On delivery day the lorry approaches from the from the A41 at its prescribed time



4. The lorry goes around the block and sits at the junction waiting at the lights ready to cross.



5. The logistical team have the gate open watching for possible bike light jumpers.



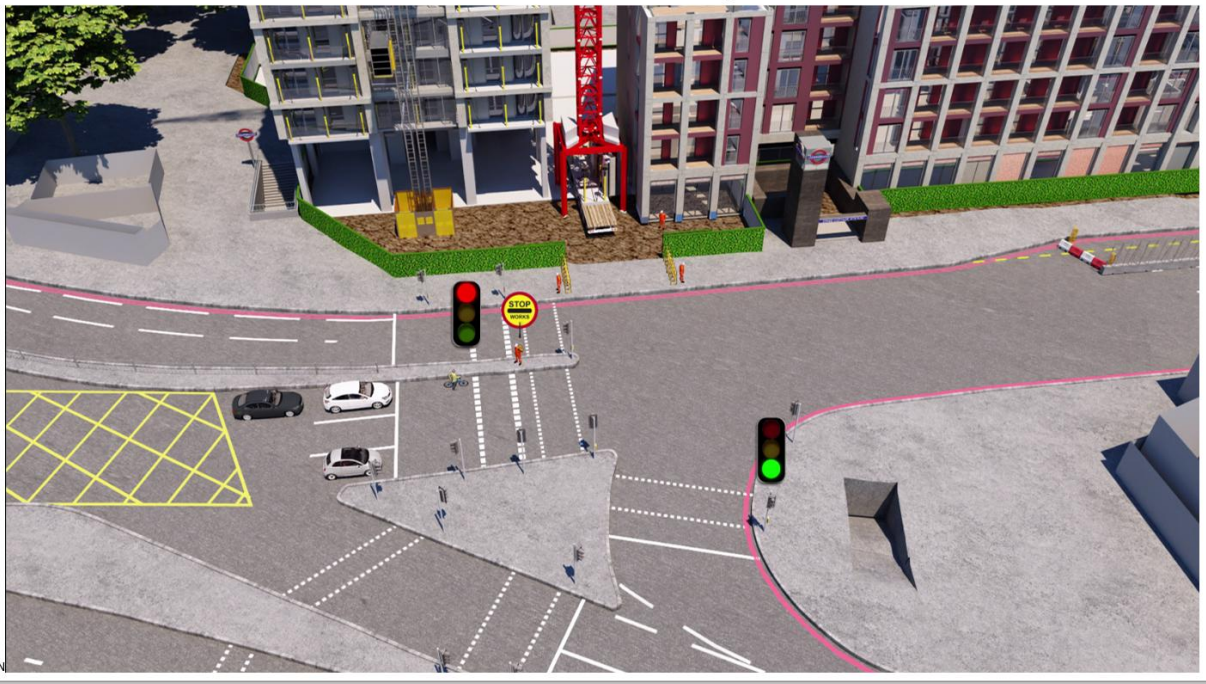
Not Confidential - Internal

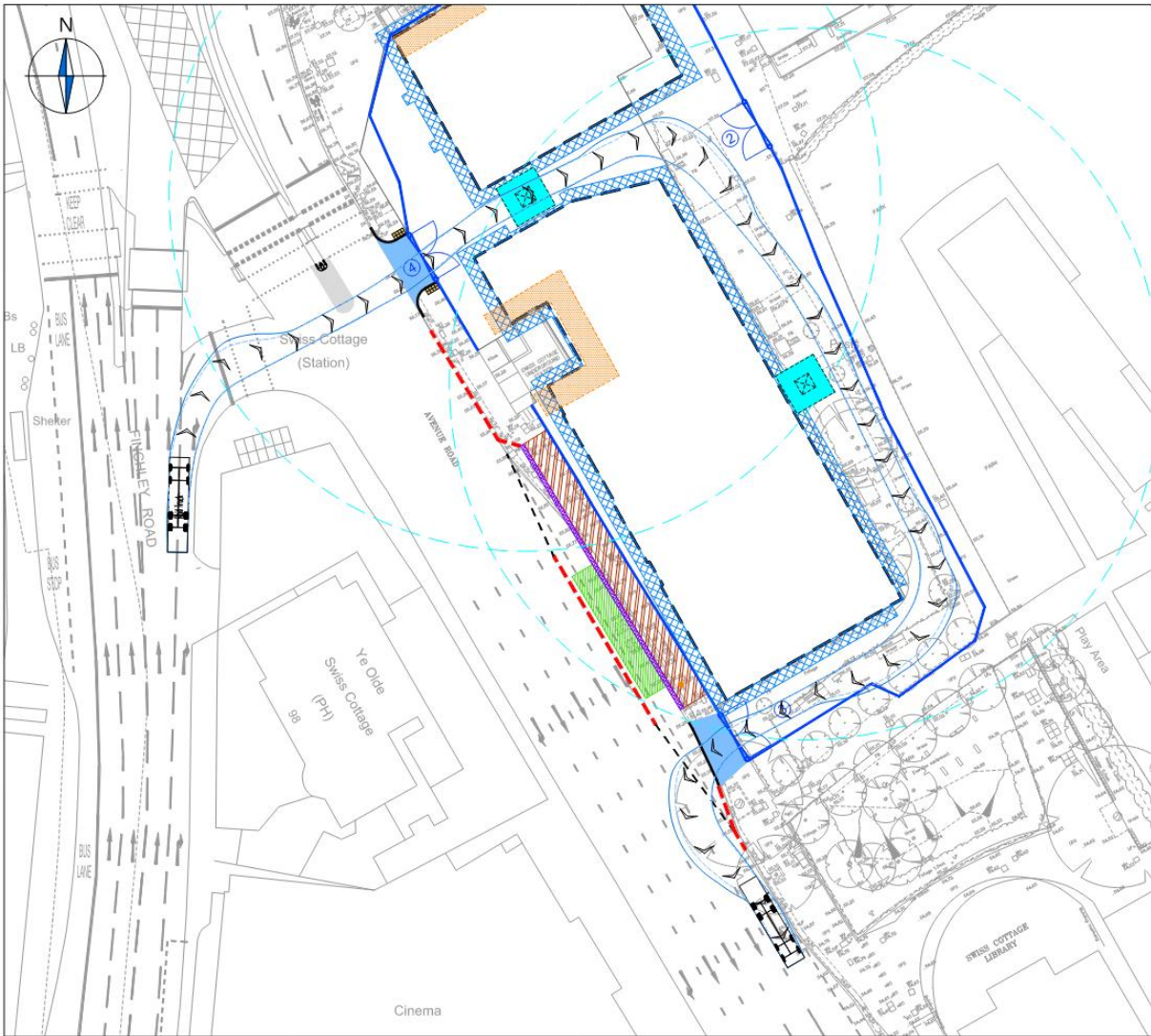
6. The lights change and the lorry is driven across in front of the B511 and onto site where the logistic team walk the vehicle through.



Not Confidential - Internal

7. The barriers are shut and the gate closed.





Route 2 - Crossover swept path analysis

b. Please confirm how contractors and delivery companies will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.

As part of our procurement process all contractors, sub contractors and suppliers will be informed of the routes to and from site. We will also establish which type of delivery vehicle will use which entrance during the various stages of construction.

We will ensure that all subcontractors and suppliers that are part of our supply chain, who have to make deliveries, will be members of Transport for London's Fleet Operator Recognition Scheme (FORS), or similar, with a minimum level of Silver. We will use our contractor selection and procurement process to only select contractors who are members of FORS (or similar), by doing this we will be using drivers who are aware of

the demands of driving large vehicles in central London, with particular awareness of cyclists and pedestrians.

Any lorries waiting to enter the site will not be allowed to idle and will be instructed to turn engines off.

By using suppliers and subcontractors who are FORS (or similar) members then all delivery vehicles will have:

- Side Guards fitted, unless it can be demonstrated to the reasonable satisfaction of the Employer, that the Lorry will not perform the function, for which it was built, if Side Guards are fitted.
- A close proximity warning system fitted comprising of a front mounted, rear facing CCTV camera (or Fresnel Lens where this provides reliable alternative), a Close Proximity Sensor, an in-cab warning device (visual or audible) and an external warning device to make the road user in close proximity aware of the driver's planned manoeuvre.
- A Class VI Mirror
- Bear prominent signage on the rear of the vehicle to warn cyclists of the dangers of passing the vehicle on the inside.

- a. Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work, including their dwell time at the site. High numbers of vehicles per day and/or long dwell times may require vehicle holding procedures.

Detailed below of the typical vehicles that will be servicing the site. In addition to the vehicles detailed in this report we will need to bring to site large items of plant, such as tower cranes – these items of plant are delivered and removed from site infrequently and will need to be pre-booked with the council and for tower crane erection there will be the need for road closure(s) which are normally over a weekend and the appropriate licenses will be sought from the Council. Cars and motorcycles are deemed infrequent and will not be counted in the delivery numbers. Vans (small delivery vehicles) will be included in the delivery numbers. Vehicle classes visiting site over the construction period will also include (pictures are representative as description and will vary) -

Category C1 vehicles weighing between 3,500kg and 7,500kg (with a trailer up to 750kg).



Category C1+E trailer entitlement to Category C1, meaning a tow trailer weighing over 750kg. The combined weight of both the vehicle and the trailer cannot exceed 1,200kg and the trailer cannot weigh more than the towing vehicle either.

Category C vehicles weighing more than 3,500kg (with a trailer up to 750kg).



Category C+E trailer entitlement to Category C, a tow trailer weighing over 750kg. no longer than 16.5m.



Category D1 minibus with no more than 16 passenger seats and a maximum length of 8 metres, and possible tow trailer up to a maximum weight of 750kg.

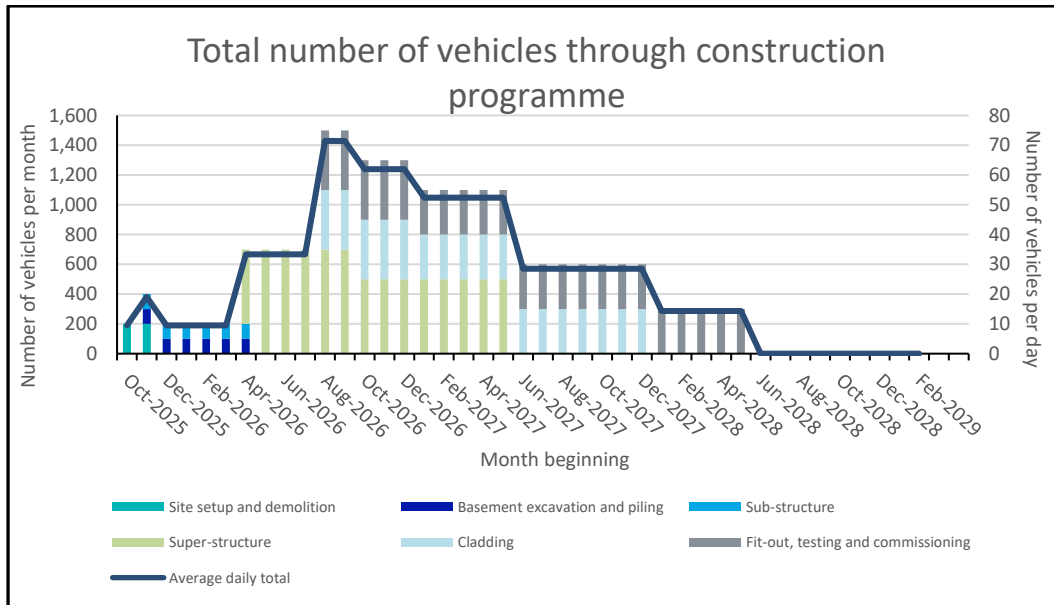
Category D1+E- D1 category vehicles with a trailer over 750kg – provided the combined weight of the trailer and vehicle do not exceed 12,000kg.

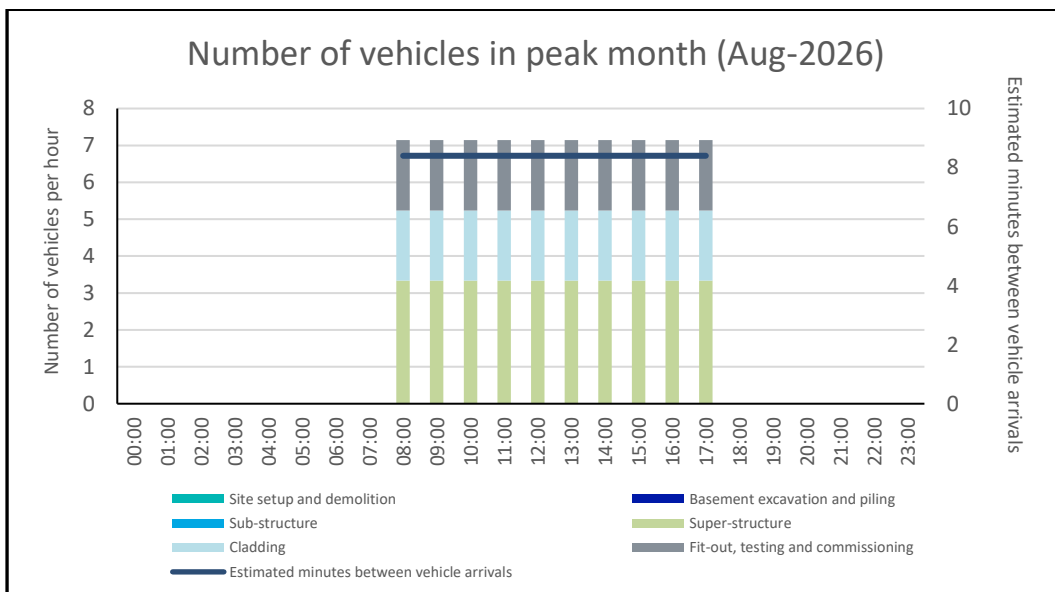
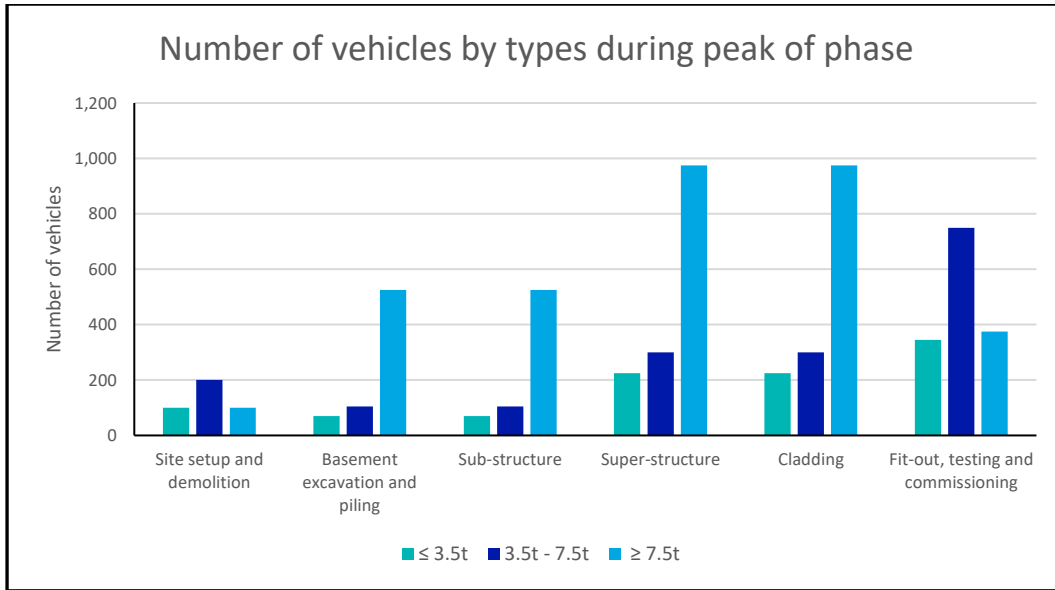
Category D+E tow trailer over 750kg on a Category D vehicle.

Category G- road roller.

Category H tracked vehicles.

Anticipated vehicle movements





The plan is based on delivery hours utilising A41 – 8am- 6pm Mon-Fri/ 8am-1pm Sat.

Only ‘planned and communicated’ use of Winchester Road -This would only be requested in exceptional circumstances for example when the site hoarding is being removed or trees being planted where access from the A41 would not be possible.

b. Please provide details of other developments in the local area or on the route.

For developments in the local area please see section 16. for developments with planning or that are under construction currently access the proposed route. The developments as they currently stand may be impacted include Hampstead Theatre and the Royal Central School of Speech and Drama and they are noted as being part of our consultation process – any outcomes from any consultation will be included in the CMP.

c. Please outline the system that is to be used to ensure that the correct vehicle attends the correct part of site at the correct time.

All deliveries will be pre booked using an online Delivery Management System to book, track and manage site deliveries. This will be controlled and managed by our on-site Logistics Manager. Additionally we will achieve this via the use of our sub-contractor coordination meetings where we will implement short term look-a-head programmes that will include the booking of deliveries.

This system will prevent potential miscommunication, double booking of gates and build-up of site traffic. The Delivery Management System will also produce a range of detailed reports including CO2 reports, FORS/ CLOCS and delivery stats.

d. Please identify the locations of any off-site holding areas (an appropriate location outside the borough may need to be identified, particularly if a large number of delivery vehicles are expected) and any measures that will be taken to ensure the prompt admission of vehicles to site in light of time required for any vehicle/driver compliance checks. Please refer to question 24 if any parking bay suspensions will be required for the holding area.

Due to the stringent delivery schedule that will be adhered throughout the project Arada don't envisage the need for consolidation centres.

The movement of all vehicles as they approach, enter and leave the site will also be controlled by traffic marshals. They will be responsible for the control and coordination of all aspects of material deliveries and movement. Vehicles will pull into the site or pit lane and never deliver on open areas outside the site. 2 tower cranes will be provided to facilitate easy and quick unloading of delivery vehicles. The cranes will be up to 50m in radius, but the use of a luffing jib will prevent the over-sailing of adjacent properties.

To minimise traffic to the western section of the site a pit lane has been implemented in the former bus stop D which will necessitate the temporary relocation of bus stop D.

Materials will be stored within the boundary of the site there will also be no parking policy on the site and all sub-contractors will be informed at the pre order meeting that the surrounding area is for resident parking only. All subcontractors will be encouraged to use public transport.

The scaffolding will be enclosed with a hoarding and we will ensure that a minimum of 2.5m of clear footpath remains open to the public along with emergency access for vehicles along the eastern edge.

The tower cranes that we propose to use will have a luffing jib, thereby eliminating the need for over-sailing, except for unloading/loading and distribution from loading bays storage areas and the pit lane.

There will also be oversailing from the mast climber during construction and public protection fans at lower level from the scaffold to protect the public at the north elevation.

e. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of [construction material consolidation centres](#)).

No other measures are being assessed at this time.

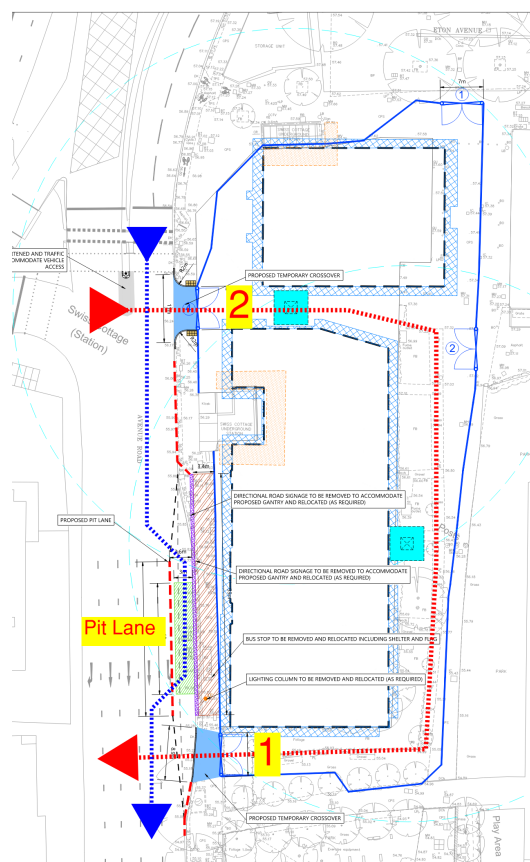
22. Site entry/exit: *“Clients shall ensure that access to and egress from the site is appropriately managed, clearly marked, understood and clear of obstacles.” (P18, 3.4.3)*

This section is only relevant where vehicles will be entering the site. Where vehicles are to load from the highway, please leave this section blank and refer to Q21. Where loading is to take place from a dedicated pit lane located on the public highway, please use this section to describe how vehicle entry/departure will be managed.

Vehicles entering and leaving the site should be carefully managed, using gates that are clearly marked and free from obstacles. Traffic marshals must ensure the safe passage of all traffic on the public highway, in particular pedestrians and cyclists, when vehicles are entering and leaving site, particularly if reversing.

Traffic marshals, or site staff acting as traffic marshals, should hold the relevant qualifications required for directing large vehicles when reversing. Marshals should be equipped with 'STOP – WORKS' signs (not STOP/GO signs) if control of traffic on the public highway is required. Marshals should have radio contact with one another where necessary.

- a. Please detail the proposed site entry and exit points on a map or diagram.



Main Site access points Gates 1 and 2 on Avenue Rd.

- b. Please describe how the entry and exit arrangements for construction vehicles in and out of the site will be managed, including the number and location of traffic marshals where applicable. If this is shown in an attached drawing, use the following space to reference its location in the appendices.

Access gate 1

A licence will be once again agreed with Camden Parks to enable usage of the strip of land to the south of the site. The Sculpture and seating have been relocated to a more suitable position to enable vehicles to enter and exit from the A41 and away from site. This will be the main site exit.

Access gate 2

Northern Crossover on site/southern crossover exit A41(Route 2). Entry to crossover vehicles will travel along Finchley Road/ A41 onto Avenue Road A41, turn right onto Adelaide Road, turn right again onto Finchley Road and a final right across into the northern crossover onto the construction project. Vehicles exit the construction project through the southern gate turning left onto Avenue Road then turning right onto Adelaide Road, turn right again onto Finchley Road and head back out of London via the A41, this will be the main entrance to site.

Pit lane

Access directly off the A41 using the existing bus stop to the west of the Site in a temporary "Pit Lane". Vehicles are parked here for unloading via the TC2 crane. A Gantry will be built over the existing footpath to ensure pedestrians could pass safely. The Bus stop has temporarily been moved.

All deliveries are to be supervised by a traffic marshal and reported to the Site Manager. All deliveries will be pre-booked so that the traffic marshals know when the delivery is coming and will take measures to ensure that the public are not affected by the delivery. The traffic marshal must be obeyed, and no phones or hands-free kits are to be used whilst driving, either on site roads or on public roads. A walkie-talkie system will be used so that the traffic marshals can communicate throughout the site.

Our Logistics Manager will plan works including vehicle movement, deliveries, temporary routes and facilities to ensure that the safety of the public is always maintained.

All deliveries will be co-ordinated and programmed to alleviate pressure on the road network. Deliveries will have to be pre-booked with site so that there are not any delivery vehicles waiting in the street. This will be achieved via the use of our weekly sub-contractor meetings and online booking system where deliveries will be planned and booked.

Delivery driver rules below:

REMEMBER

**VEHICLES ON SITE KILL MANY CONSTRUCTION WORKERS EVERY YEAR
BE AWARE OF PEDESTRIANS ON SITE AND PLEASE BE CONSIDERATE**

VEHICLE RULES TO BE COMPLIED WITH AT ALL TIMES

- Site speed limit of 5mph, or lower where posted
- All vehicles must be registered to CLOCS or FORS
- No reversing on site without the specific instruction of a traffic marshal
- Mandatory PPE – Hard Hat, Hi-Viz, Safety Boots (with ankle support), and Gloves, Glasses must be worn outside of cab
- Only park where instructed. Do not stop on side roads
- Bad language is prohibited as we are in close proximity to the public
- Do not use mobile phones when driving unless with hands free kit
- Obey all road traffic signage and wear seatbelts at all times
- No offloading without authorisation from Traffic Marshal
- No animals, children or unauthorised passengers permitted onto site
- Zero tolerance to Drugs & Alcohol
- No smoking, eating or drinking on site or in vehicle cabs
- Keys must not be left in unattended vehicles
- When fitted, orange beacons must be used when on site
- Fall prevention equipment required if climbing on to vehicle
- COVID 19 and Social Distancing Guidelines must be followed at all times
- Skip Drivers must hold an A39 CPCS Qualification – Yes / No / NA
- 12 Month Thorough Examination Certificate for Skip Vehicle – Yes / No / NA



traffic

NB: compliance with the above is monitored by the site traffic management team

Vehicle Registration	Date/Time
	/ / :

Do you suffer from or have you ever been affected by the following: -? Chest pains or Angina, hearing problems, visual defects, back or spinal problems, physical handicap or injury, diabetes, asthma, epilepsy or fits, dermatitis or reddened skin, alcoholism, claustrophobia, vertigo, other serious illness or occupational diseases?		Yes	No
Are you currently taking any prescription medicine?			
Drivers Name	Company	Signature	

Access from the A41 will be as per Camden Guidelines of construction vehicle movements between 7.00am to 6.00pm on weekdays and between 7.00am and 1.00pm on Saturdays. The working time periods for the Pit Lane are based on the London Borough of Camden’s standard working hours for construction sites (0800-1800 hrs Monday to Friday and 0800-1300 hrs on Saturday). The proposed additional hour in the morning (0700-0800 hrs) would allow the first two construction vehicles to be ready for loading/ unloading when the site opens.

All suppliers and sub-contractors who are supplying materials to the site will be issued with a transport plan which will include a prescribed route into the site to deliver materials from the Transport for London Road Network; refer to plans earlier in this CMP. As part of our plans to mitigate the impact of the project and its deliveries on the road network we will in the first instance look to our supply chain to store materials off site and only deliver the materials when they are needed.

c. Please provide tracking/swept path drawings for vehicles entering/exiting the site if necessary. If these are attached, use the following space to reference their location in the appendices.

Swept path analysis drawings for vehicles entering and exiting the site are provided in Appendix A.

d. Provision of wheel washing facilities should be considered if necessary. If so, please provide details of how this will be managed and any run-off controlled. Please note that wheel washing should only be used where strictly necessary, and that a clean, stable surface for loading should be used where possible.

A wash down slab will be installed at site Access points 1 & 2 in order to keep roads and footpaths free from deposits of soil, mud and the like.

We will ensure that the wheels of any vehicles leaving this site are thoroughly cleaned and hosed down prior to going on the public roads. If any mud or construction debris does get onto the street within the vicinity of the site, then these areas will be cleaned via the use of water hoses and manually swept. In addition, a mechanical road sweeper will be used to clear any debris.

23. Vehicle loading and unloading: *“Clients shall ensure that vehicles are loaded and unloaded on-site as far as is practicable.” (P19, 3.4.4)*

If this is not possible, Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and motor traffic in the street when vehicles are being loaded or unloaded.

Please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g., delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will access and egress the site. If loading is to take place off site, please identify where this is due to take place and outline the measures you will take to ensure that loading/unloading is carried out safely. Please outline in question 24 if any parking bay suspensions will be required.

The logistics plan and swept path plan demonstrate how all access to the site will take place and all unloading will be from the protected pit lane or within the site.

All materials will be stored within the site.

All skips will be located within the site.

Highway interventions

Please note that Temporary Traffic Orders (TTOs) and hoarding/scaffolding licenses may be applied for prior to CMP submission but won't be granted until the CMP is signed-off.

If the site is on or adjacent to the TLRN, please provide details of preliminary discussions with Transport for London in the relevant sections below.

24. Parking bay suspensions and temporary traffic orders

Please note, parking bay suspensions should only be requested where absolutely necessary. Parking bay suspensions are permitted for a maximum of 6 months, requirement of exclusive access to a bay for longer than 6 months you will be required to obtain [Temporary Traffic Order \(TTO\)](#) for which there is a separate cost.

Please provide details of any proposed parking bay suspensions and TTO's which would be required to facilitate construction. **Building materials and equipment must not cause obstructions on the highway as per your Considerate Contractors obligations unless the requisite permissions are secured.**

Information regarding parking suspensions can be found [here](#).

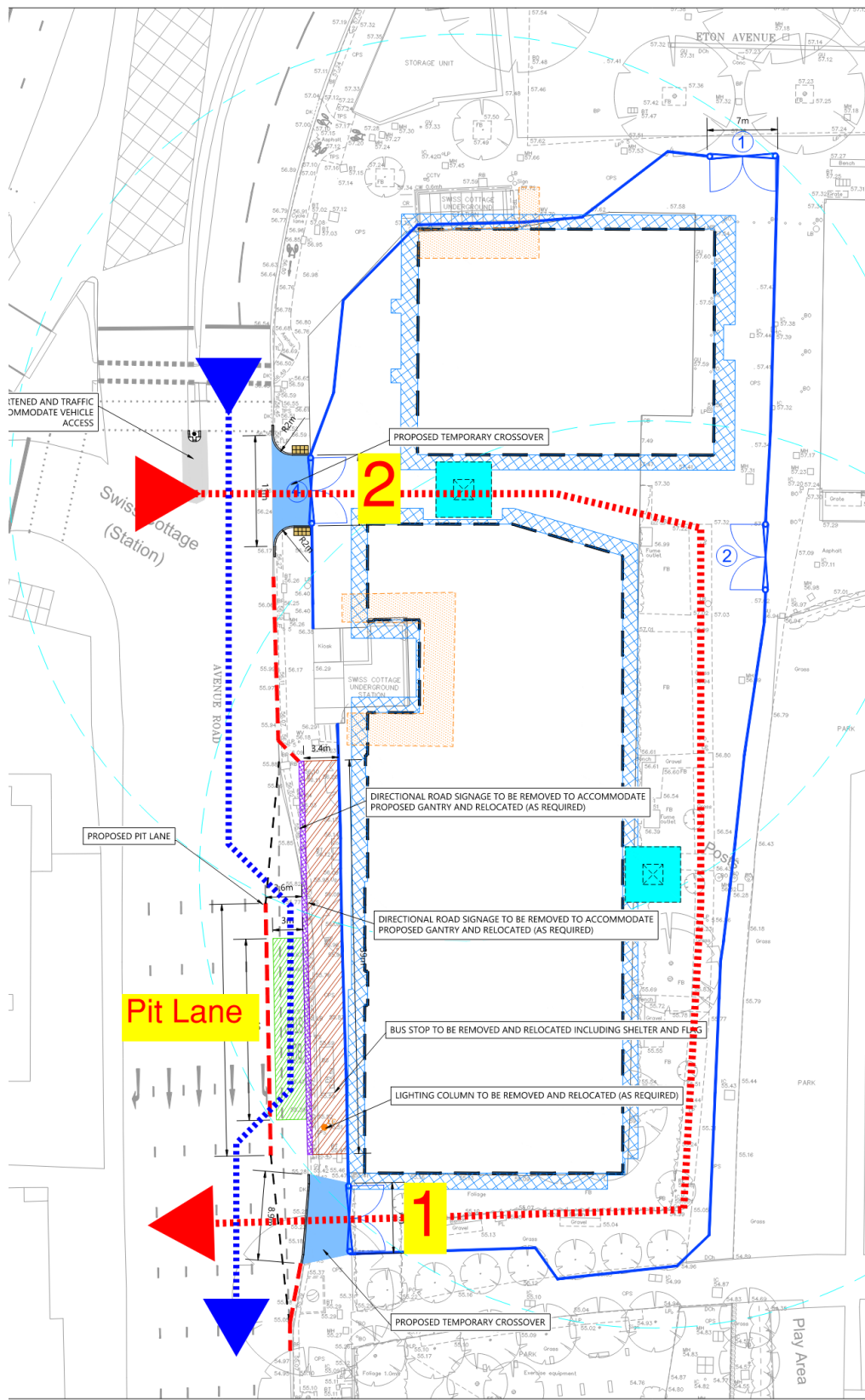
Currently Arada do not foresee the need for any parking bay suspension as the current transport routes have taken current parking bays into account.

25. Scaled drawings of highway works

Please note that use of the public highway for storage, site accommodation or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose such use you must supply full justification, setting out why it is impossible to allocate space on-site. You must submit a detailed (to-scale) plan showing the impact on the public highway that includes the extent of any hoarding, pedestrian routes, parking bay suspensions and remaining road width for vehicle movements. We prefer not to close footways but if this is unavoidable, you should submit a scaled plan of the proposed diversion route showing key dimensions.

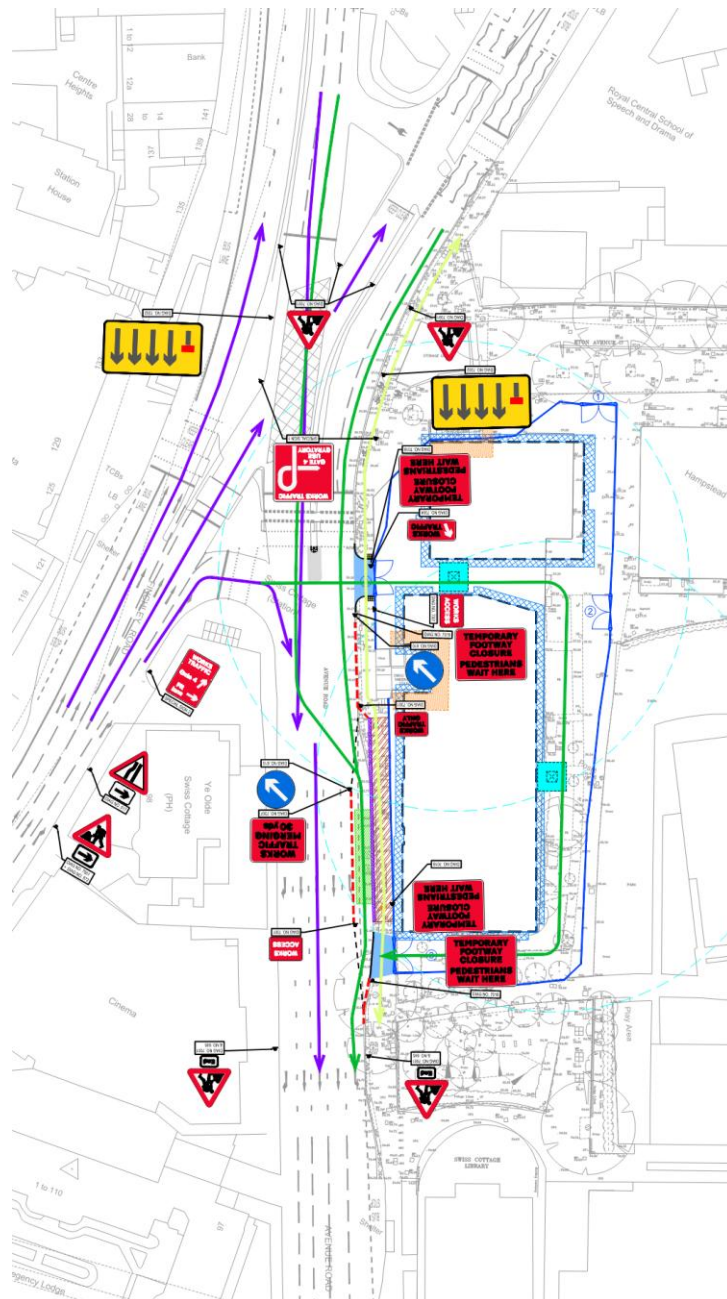
a. Please provide accurate scaled drawings of any highway works necessary to enable construction to take place (e.g. construction of temporary vehicular accesses).

Refer to the logistics plan below.



b. Please provide details of all safety signage, barriers and accessibility measures such as ramps and lighting etc.

Regal Construction will deploy and use all necessary and appropriate safety signage and barriers to ensure that the public are protected and our operatives and work safely at all times.



26. Diversions

Where applicable, please supply details of any diversion, disruption or other anticipated use of the public highway during the construction period (alternatively a plan may be submitted).

Bus stop D will be re-diverted to allow the erection of the pit lane and gantry for the duration of the construction period.

We also need to introduce the second crossover to the north of the LUL Avenue to facilitate deliveries from Avenue Road. We will be making separate highway applications to Camden close sections of the footpath and will be agreeing traffic management with TfL.

The entrance exit to the south of the site has been agreed and installed by Essential Living. A new park license will need to be agreed in due course.

We are aware of the existing utility services around the site and there will be disconnections and diversions. These will be planned to have minimum impact on the highway and the public.

27. VRU and pedestrian diversions, scaffolding and hoarding

Pedestrians and/or cyclist safety must be maintained if diversions are put in place. Vulnerable footway users should also be considered. These include wheelchair users, the elderly, those with walking difficulties, young children, those with prams, the blind and partially sighted. Appropriate ramping must be used if cables, hoses, etc. are run across the footway.

Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions. Lighting and signage should be used on temporary structures/skips/hoardings etc.

A secure hoarding will generally be required at the site boundary with a lockable access.

- a. Please provide details describing how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Traffic Marshall arrangements.

When vehicles are entering or leaving the site, these will be supervised by our traffic marshals. Where possible vehicles will be unloaded from within the sites but where this

is not possible off loading areas will be defined and marshalled – these are noted on the site logistics plans attached.

The general public/pedestrians will have the right of way along the pathways that surround the site. At crossover, concertina barriers will be used to protect pedestrians from vehicles entering site.



The construction site gates will be kept closed and monitored by site security, only when deliveries are made to the site will they be opened to allow vehicles onto the site, at which time barriers will be used to prevent access by pedestrians and warn any passing cyclists. These barriers will be manned by our site security. All delivery vehicles will be supervised/controlled by a traffic marshall.

The Site Manager will also ensure that the external perimeter of the site is regularly patrolled to ensure that any debris is kept clear of the pavements.

Should there be any complaints arising from the works, local residents will be able to call personally to the site offices. Any residents visiting site to raise a complaint will be requested to sign-in and our security guard will escort the visitor to the site offices. There will also be an online complaints submission forum available for members of the public raise there concerns.

We will, as part of our sub-contractor procurement process, ensure that all sub-contractors and suppliers delivering materials to the site follow the conditions outlined in the Standard for Construction Logistics and Community Safety (CLOCS).

The Main Contractor will be obliged to appoint a director level point of contact who will deal personally with comments or complaints from the public or neighbours and will

ensure that they are resolved swiftly. A record will be kept of all comments and complaints.

All pedestrian within the gantry area will be protected by solid hoarding. When pedestrians are crossing site gates and marshals will only cordon the crossings when needed to make it safe to cross.

b. Please provide details of any temporary structures which would overhang the public highway (e.g. scaffolding, gantries, cranes etc.) and details of hoarding requirements or any other occupation of the public highway.

All hoardings to be a minimum of 2.4m high with suitable lighting provided from dawn till dusk except for the splayed corner at the NW of the site.

The pit lane gantry will be detailed and sits to the west of the project.

A protection gantry will also be provided to both LUL access points around the project.

Due the heavily undulating land to the rear of the site the hoarding will be set to a level to ensure that all members of the public are protected.

Hoarding, scaffold and the gantry licenses will be applied for at Camden and TfL.

Environment

To answer these sections please refer to the relevant sections of **Camden's Minimum Requirements for Building Construction (CMRBC)**.

28. Please list all noisy operation_ and the construction methods used, and provide details of the times that each of these are due to be carried out.

By its nature construction works can cause noise, noise being created by; mechanical plant, cutting, drilling, hammering and sawing. All noisy work will be restricted to occur only after 8.00am and before 6.00pm Monday to Friday (excluding Bank Holidays). We will always seek to not carry out noisy work on a Saturday when we are permitted to work between 8am and 1pm.

The activities that will create “noisy” operation are:

- Breaking and drilling of concrete
- Concrete lorries, Concrete pumps, hoists, steelwork etc.
- Drilling & the use of nail guns during the fit out

To reduce noise we will:

- Adhere to working hours
- Plan deliveries by arranging routes and times to minimise potential nuisance to the local community
- Keep noisy plant away from public areas
- Minimise drop heights into hoppers, lorries and other plant
- Use local screening where necessary
- Screen is placed between plant and a nearby sensitive location, ie house
- Silenced generators and tower lights where necessary
- Keep acoustic doors, hoods and panels on plant closed – it does make a difference
- Minimise vehicles and plant reversing – incorporate one way systems

Due to the noise, sensitive nature of the some of the surrounding receptors specifically the Hampstead Theatre to the north side of the site. Quiet periods will be arranged along the north side of the site in line with their performance schedule, we will be consulting with the Theatre managers to agree any times where we need to reduce site noise.

29. Please confirm when the most recent pre-construction noise survey was carried out and provide a copy. If a noise survey has not taken place, and it has been requested by the local

authority, please indicate the date (before any works are being carried out) that the noise survey will be taking place, and agree to provide a copy.

The most recent noise survey was undertaken by RBA between 15 May 2024 and 20 May 2024 and is attached at Appendix E.

30. Please provide predictions for noise levels throughout the proposed works.

Please refer to the Construction Environmental Management Plan – Noise Predictions document in Appendix F.

31. Please provide details describing mitigation measures to be incorporated during the construction works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

We will ensure that all plant has noise reduction or has an acoustic blanket around the plant.

Noise levels are monitored, should any exceedances trigger an alert our logistics company will be made aware, investigate the issue and carry out measures to reduce the noise.

32. Please provide evidence that staff have been trained on BS 5228:2009

All Project Managers will be required to attend the Site Managers Safety Training Scheme as run by the CITB. All sub-contractors supervisors will have attended the Site Supervisors Safety Training Scheme as run by the CITB.

Noise awareness will be cascaded via toolbox talks.

33. Please provide specific details on how air pollution and dust nuisance arising from dusty activities on site will be prevented. This should be relevant and proportionate to activities due to take place, with a focus on both preventative and reactive mitigation measures.

Arada and its contractors will adhere to the key legislation and site-specific restrictions as detailed in the:

- Control of Pollution Act 1974
- Environmental Protection Act 1990 (ss79-82)
- BS 5228:1997, Code of Practice on Construction and Open Site.
- Camden's Minimum Requirements (CMR230772,)(Addendum233259 CMR230772)
- British Standards BS5228:2009+A1:2014

- The Control of Dust and Emissions During Construction and Demolition (SUPPLEMENTARY PLANNING GUIDANCE) 2014
Noise/vibration reduction and visible dust prevention philosophies.
- 'O'Keefe Demolition' Document Reference D001-NV-001 'Control of Demolition Noise and Vibration 100 Avenue Road Swiss Cottage' dated 20th August 2017 and further revisions.
- Property and Infrastructure Capita 'Baseline Noise Monitoring' reference 296993-01 (00) dated November 2017 and further revisions.

Site operations will be controlled so that all plant and machinery noise emissions (including the provision of ventilation, heating and cooling) shall be designed, installed and operated at noise levels that do not cause noise nuisance to the nearest adjoining residential properties.

We have, with our sub-contractors, implemented a Dust Management Plan (DMP) that has previously been approved by Camden. The DMP will includes but not be limited to:

- Only use cutting, grinding or sawing equipment fitted or in conjunction with suitable dust suppression techniques such as water sprays or local extraction
- Consider full enclosure of sites or specific operations where there is a high potential for dust production and the site is active for an extensive period
- Where reasonably practicable store materials with the potential to produce dust away from site boundaries
- Ensure no burning of waste materials takes place on site
- Do not allow dry sweeping
- Ensure regular cleaning of hard standings using wet sweeping methods
- Maintain all dust control equipment in good condition and record maintenance activities
- Routinely clean public roads and access routes using wet sweeping methods
- Ensure all vehicles carrying loose or potentially dusty material to or from the site are fully sheeted
- Ensure that any crushing or grinding plant used on the site, which falls within the definition in Section 3.5 Chapter 3 of the Pollution Prevention and Control (England and Wales) Regulations 2000 SI 1973, has an appropriate permit issued and is maintained According to the procedures set out in the Pollution, Prevention and Control Act 1999.

We are aware of the Dust & Air Emissions Mitigation Measures as prepared by the Institute of Air Quality Management and will use their mitigation and control measure to ensure that dust is controlled on site both during demolition and construction.

34. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

Dust and debris on the road are one of the main environmental nuisance and safety problems arising from construction sites.

Arada have instigated provisions to minimise this problem by having a wheel wash jet on site and the site access road will also be a tarmac construction.

The wash slab area located at the exit from this site will be impermeable and isolated from the surrounding area by a raised kerb or roll over bund to contain solids, with effluent directed to the foul sewer

We will also make provision for cleaning of the road by an approved road sweeper.

We will use a fine spray to suppress dust on the following:

- Unpaved areas that are subject to traffic or wind.
- Sand, spoil and aggregate stockpiles.
- During loading/unloading of dust generating materials

35. For medium or high impact risk level sites, please provide details describing arrangements for monitoring of noise, vibration and dust levels, including instrumentation, locations of monitors and trigger levels where appropriate.

Please refer to Appendix M (Air Quality and Dust Management Plan)

36. Please confirm that an Air Quality Assessment and/or Dust Risk Assessment has been undertaken at planning application stage in line with the GLA policy [The Control of Dust and Emissions During Demolition and Construction 2014 \(SPG\)](#) (document access at bottom of webpage), and that the summary dust impact risk level (without mitigation) has been identified. The risk assessment must take account of proximity to all human receptors and sensitive receptors (e.g. schools, care homes etc.), as detailed in the [SPG](#). **Please attach the risk assessment and mitigation checklist as an appendix.**

Please refer to Appendix M (Air Quality and Dust Management Plan)

37. Please confirm that all of the GLA's 'highly recommended' measures from the SPG document relative to the level of dust impact risk identified in question 36 have been addressed by completing the GLA mitigation measures checklist. (See Appendix 7 of the SPG document.)

The measures listed in section 2 of the DMP are all of the GLA's 'highly recommended' measures from the SPG document relative to the level of dust impact risk identified.

38. Please confirm the number of real-time dust monitors to be used on-site.

Note: **real-time dust (PM₁₀) monitoring with MCERTS 'Indicative' monitoring equipment will be required for all sites with a high OR medium dust impact risk level.** If the site is a 'high impact' site, 4 real time dust monitors will be required. If the site is a 'medium impact' site', 2 real time dust monitors will be required.

The dust monitoring must be in accordance with the SPG and IAQM guidance, and **the proposed dust monitoring regime (including number of monitors, locations, equipment specification, and trigger levels) must be submitted to the Council for approval.** Dust monitoring is required for the entire duration of the development and must be in place and operational **at least three months prior to the commencement of works on-site.** Monthly dust monitoring reports must be provided to the Council detailing activities during each monthly period, dust mitigation measures in place, monitoring data coverage, graphs of measured dust (PM₁₀) concentrations, any exceedances of the trigger levels, and an explanation on the causes of any and all exceedances in addition to additional mitigation measures implemented to rectify these.

In accordance with Camden's Clean Air Action Plan, the monthly dust monitoring reports must also be made readily available and accessible online to members of the public soon after publication. Information on how to access the monthly dust monitoring reports should be advertised to the local community (e.g. presented on the site boundaries in full public view).

Inadequate dust monitoring or reporting, or failure to limit trigger level exceedances, will be indicative of poor air quality and dust management and will lead to enforcement action.

The site has been identified in the Dust Risk Assessment as low risk for construction, and medium risk for trackout. As a result, two PM₁₀ monitors will be operated, as set out in Section 4 of the DMP. It is confirmed that these monitors will meet MCERTs standard. Further details of the location, equipment and trigger levels and report set out in the DMP. As there is long term PM₁₀ monitoring data from the Swiss Cottage air quality monitor, which is 80m from the development site, it is not considered necessary to gather 3-months of monitoring data prior to commencement of the construction works. This approach is consistent with the approach in IAQMs Guidance on Monitoring in the Vicinity of Demolition and Construction Sites, which states that, *“In most situations, baseline monitoring may not be required, e.g. in some urban areas where there is a large existing body of monitoring data (and where these sites are expected to continue to operate throughout the duration of the construction works).”* Monthly dust reports will be provided to Camden.

39. Please provide details about how rodents, including rats, will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and present copies of receipts (if work undertaken).

If there is evidence of rodents following regular surveys then a qualified pest control firm will follow the procedures set out by the HSE in Information Sheet MISC515 for the laying of baits. The baits being approved under the Control of Pesticides Regulations 1986 (as amended 1997)/Biocidal Products Regulations 2001 (as amended 2003). As part of the work by a qualified pest control firm will be employed.

All further works will be completed in accordance with “Pest minimisation Best Practice for the Construction Industry” for eradication of rat/mice.

40. Please confirm when an asbestos survey was carried out at the site and include the key findings.

An asbestos survey was carried out in September 2014. All works relating to this have now been completed.

The below table summarises the asbestos containing materials identified during the inspection that was removed.

Floor	Location	Material	Level of ID	Asbestos Type	Action
5th	Risers 2, 3 & 4	Rope expansion	Identifi	Chrvsotile	Remove
		between			HSE guidelines
		walls & concrete			

		ceili			
4th	Risers 2, 3 & 4	Rope expansion between walls & concrete ceili	Strongl presum	Chrysotile	Remove HSE guidelines
3rd	Risers 2, 3, 4, 5	Rope expansion between walls & concrete ceili	Strongl presum	Chrysotile	Remove HSE guidelines
2nd	Risers 2, 3, 4 &	Rope fire stop to of block built riser	Identifi	Chrysotile	Remove HSE guidelines
1st	Risers 2, 3, 4 &	Rope fire stop to of block built riser	Strongl presum	Chrysotile	Remove HSE guidelines
Ground	Risers 2, 3 & 5	Rope fire stop to of block built riser	Strongl presum	Chrysotile	Remove HSE guidelines
Baseme	Boiler room	Rope gasket to penetration	Identifi	Chrysotile	Remove HSE guidelines
Baseme	Boiler room	Rope gaskets boiler units	Identifi	Chrysotile	Remove HSE guidelines
Baseme	Store	Rope gasket to penetration	Strongl presum ed	Chrysotile	Remove HSE guidelines

41. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of a suitable smoking area, tackling bad language and unnecessary shouting.

Arada take seriously the conduct of the builders on all of our sites to ensure that there is not any anti-social behaviour.

Should there be any complaints arising from the works, local residents will be able to call personally to the site offices. Any residents visiting site to raise a complaint will be requested to sign-in and our security guard will escort the visitor to the site offices.

Our Community Liaison Manager will deal personally with comments or complaints from the public or neighbours and will ensure that they are resolved swiftly. A record will be kept of all comments and complaints.

There will also be a project web site where local residents will be able to comment on any issues with the construction.

At Arada we do not tolerate any bad language or unnecessary shouting on our sites. We operate a “red card” system whereby any operative found to be acting in an anti-social way or smoking outside of designated smoking area will be given a “red card” and asked to leave the site immediately.

No radios will be allowed on site.

42. If you will be using non-road mobile machinery (NRMM) on site with net power between 37kW and 560kW it will be required to meet the standards set out below. The standards are applicable to both variable and constant speed engines and apply for both PM and NOx emissions.

Arada confirm that we will register the project with NRMM and all applicable non road plant will be logged and checked to ensure that its emission levels are below the targets set by the NRMM Register and best practice established in Camden set out in the table below. Any machinery and plant to exceed the required emission levels will not be allowed onto the site.

Contractor HGVs	100% Euro VI. Standards adopted from start of scheme.	Exemptions: specialist vehicles where Euro VI demonstrated unfeasible will be permitted up to a maximum of 8% of unique vehicles over course of scheme.	Contractors to set out their exemptions & compliance management process in LESSMP and report to Applicant on monthly basis, including vehicle numbers and any non-compliances. Once continuous improvement is shown in each of three separate months, frequency will be reduced.
Contractor Cars	Euro IV (petrol) or Euro VI (diesel) or ultra low emission vehicles. Adopting standards from beginning of construction, reach 100% by 1 st January, 2020	Exemptions: None permitted	
Contractor Vans	Euro IV (petrol) or Euro VI (diesel) or ultra low emission vehicles. Adopting standards from beginning of construction, reach 75% by 1 st January, 2020	Exemptions: 25% of unique vehicles permitted after above milestone.	

From 1st September 2015

(i) Major Development Sites – NRMM used on the site of any major development will be required to meet Stage IIIA of EU Directive 97/68/EC

(ii) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IIIB of EU Directive 97/68/EC

From 1st September 2020

(iii) Any development site - NRMM used on any site within Greater London will be required to meet Stage IIIB of EU Directive 97/68/EC

(iv) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IV of EU Directive 97/68/EC

Please provide evidence demonstrating the above requirements will be met by answering the following questions:

- a) Construction time period (mm/yy - mm/yy): 01/26 – 10/28
- b) Is the development within the CAZ? (Y/N): No
- c) Will the NRMM with net power between 37kW and 560kW meet the standards outlined above? (Y/N): Yes
- d) Please confirm that all relevant machinery will be registered on the NRMM Register, including the site name under which it has been registered: The project will be registered with the NRMM as noted in question 42.
- e) Please confirm that an inventory of all NRMM will be kept on site and that all machinery will be regularly serviced and service logs kept on site for inspection: An inventory will be kept on site including service log records – all in accordance with the NRMM Register.
- f) Please confirm that records will be kept on site which details proof of emission limits, including legible photographs of individual engine plates for all equipment, and that this documentation will be made available to local authority officers as required: This requirement will be actioned on site.

43. Vehicle engine idling (leaving engines running whilst parked or not in traffic) produces avoidable air pollution and can damage the health of drivers and local communities. Camden Council and the City of London Corporation lead the London **Idling Action Project** to educate drivers about the health impacts of air pollution and the importance of switching off engines as a simple action to help protect the health of all Londoners.

Idling Action calls for businesses and fleet operators to take the **Engines Off pledge** to reduce emissions and improve air quality by asking fleet drivers, employees and subcontractors to avoid idling their engines wherever possible. Free driver training materials are available from the website: <https://idlingaction.london/business/>

Please provide details about how you will reduce avoidable air pollution from engine idling, including whether your organisation has committed to the Engines Off pledge and the number of staff or subcontractors who have been provided with free training materials.

TBC

Mental Health Training

44. Poor mental health is inextricably linked to physical health, which in turn impacts performance and quality, and ultimately affects productivity, creativity and morale. Workers in the construction industry are six times more likely to take their own life than be killed in a fall from height.

We strongly recommend signing up to the “[Building Mental Health](#)” charter, an industry-wide framework and charter to tackle the poor mental health in the construction industry, or joining [Mates In Mind](#), which providing the skills, clarity and confidence to construction industry employers on how to raise awareness, improve understanding and address the stigma that surrounds mental health.

The Council can support by providing free Mental Health First Aid training, publicity resources and signposting to local support services.

Please state whether you are or will be signed up to the Building Mental Health charter (or similar scheme), and that and appropriate number of trained Mental Health First Aiders will be available on site.

TBC

Agreement

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed in writing by the Council. This may require the CMP to be revised by the Developer and reapproved by the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council in writing and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

Signed:

Date:

Print Name:

Position:

Please submit to: planningobligations@camden.gov.uk

End of form.

V2.9

Appendix A

Appendix B

Appendix C

Appendix D

Appendix E

Appendix F

Appendix H

Appendix M

Appendix N

Appendix O

Appendix P

Appendix Q

Appendix S

Appendix V

Appendix W

Appendix X